

ROAD TRANSPORT (GENERAL) ACT 2005

4.6 Metre High Vehicle Notice 2013

I, Peter Duncan, Chief Executive of Roads and Maritime Services, pursuant to Clause 10 of Schedule 2 to the *Road Transport (Vehicle Registration) Regulation 2007*, and Clauses 12, 20 and 25 of the *Road Transport (Mass, Loading and Access) Regulation 2005*, hereby exempt vehicles that are described in Part 2 of the Schedule to this Notice from the dimensions relating to height as set out in Clause 77 (1) of Schedule 2 to the *Road Transport (Vehicle Registration) Regulation 2007* and Clause 8(1) of Schedule 1 to the *Road Transport (Mass, Loading and Access) Regulation 2005* while on the areas or routes specified in Part 3 of the Schedule to this Notice subject to the conditions and requirements set out in the Schedule to this Notice.

Peter Duncan
Chief Executive
Roads and Maritime Services

SCHEDULE

PART 1 - PRELIMINARY

1.1 Citation

This Notice may be cited as the *4.6 Metre High Vehicle Notice 2013*.

1.2 Commencement

This Notice takes effect on 1 January 2013.

1.3 Effect

This Notice remains in force until 31 December 2017 unless it is amended or repealed earlier.

1.4 Interpretation

- (a) Unless stated otherwise, words and expressions used in this Notice have the same meaning as those set out in the *Road Transport (General) Act 2005*.
- (b) Unless stated otherwise, notes in this Notice do not form part of this Notice.

PART 2 - VEHICLE CLASSES

For the purposes of this Notice the following classes of vehicles are specified.

2.1 Class 1 vehicles

- (a) A special purpose vehicle that exceeds 4.3 metres, but does not exceed 4.6 metres, in height;

- (b) A vehicle or combination (including a low loader or load platform combination) that is specially designed for the carriage of a large indivisible item, or is carrying a large indivisible item, that together with any load, exceeds 4.3 metres but does not exceed 4.6 metres in height;

2.2 Class 2 vehicles

- (a) A combination carrying vehicles on more than one deck that together with any load, exceeds 4.3 metres but does not exceed 4.6 metres in height;
- (b) A single motor vehicle, or a combination, that exceeds 4.3 metres but does not exceed 4.6 metres in height and is built to carry cattle, sheep, pigs or horses.

2.3 Class 3 vehicles

- (a) A single motor vehicle, or a combination, that, together with its load exceeds 4.3 metres but does not exceed 4.6 metres in height and is carrying wool, hay bales or other primary produce;
- (b) A single motor vehicle carrying vehicles on more than one deck that, together with its load exceeds 4.3 metres but does not exceed 4.6 metres in height.
- (c) A single motor vehicle, or a combination, that is constructed to exceed 4.3 metres in height, but does not exceed 4.6 metres in height and is carrying freight, other than cattle, sheep, pigs, horses, wool, hay bales, or other primary produce.
- (d) A single motor vehicle, other than a double deck bus, or a combination, that together with any load exceeds 4.3 metres in height, but does not exceed 4.6 metres in height.
- (e) A double deck bus that exceeds 4.3 metres in height, but does not exceed 4.4 metres in height.
- (f) A single motor vehicle or combination carrying a freight container that together with its load exceeds 4.3 metres in height, but does not exceed 4.6 metres in height.

PART 3 - VEHICLE ACCESS

3.1 Approved 4.6m high routes and areas

The vehicles described in Part 2 of this Notice may operate on the routes and in the areas specified in Appendix 1 if they comply with Part 4 and the relevant sections of Part 5 of this Notice.

Note: Approved 4.6 metre high vehicle areas and routes are assessed for their suitability, however over time routes may be obstructed by vegetation or temporary works. Responsibility for due care and diligence remains with the individual driver to avoid any such obstruction.

Note: For the 'Special Requirements' relating to construction, loading and equipment refer to Clause 5.1 (general freight); 5.2 (stock crates); 5.3 (car carriers); 5.4 (hay bales); 5.5 (wool bales); 5.6 (freight containers).

3.2 B-doubles travelling on 4.6m high routes

A B-double that exceeds 4.3 metres but does not exceed 4.6 metres in height may operate on routes and in the areas permitted in this Notice if:

- (a) it is also permitted to do so by the *Class 2 B-Double Notice 2010* as amended from time to time; and
- (b) any relevant conditions set out in the *Class 2 B-Double Notice 2010* as amended from time to time, are complied with.

Note: This means the road must be approved for both 4.6m high and B-double travel.

3.3 Road Trains travelling on 4.6m high routes

A road train that exceeds 4.3 metres but does not exceed 4.6 metres in height may operate on the routes and in those areas permitted in this Notice if:

- (a) it is also permitted to do so by the *Class 2 Road Train Notice 2012* as amended from time to time; and
- (b) any relevant conditions set out in the *Class 2 Road Train Notice 2012* as amended from time to time, are complied with.

Note: This means the road must be approved for both 4.6m high and road train travel.

3.4 Modular B-triples travelling on 4.6m high routes

A modular B-triple that exceeds 4.3 metres but does not exceed 4.6 metres in height may operate on the routes and in those areas permitted in this Notice if:

- (c) it is also permitted to do so by the *Class 2 Modular B-triple Notice 2012* as amended from time to time, and
- (d) any relevant conditions set out in the *Class 2 Modular B-triple Notice 2012* as amended from time to time, are complied with.

Note: This means the road must be approved for both 4.6m high and modular B-triple travel.

3.5 Car Carriers travelling on 4.6m high routes

A car carrier that exceeds 4.3 metres but does not exceed 4.6 metres in height may operate on the routes and in those areas permitted in this Notice if:

- (a) it also permitted to do so by the *Class 2 Car Carrier Combination Notice 2011* as amended from time to time.
- (b) any relevant conditions set out in the *Class 2 Car Carrier Combination Notice 2011* as amended from time to time, are complied with.

Note: This means the road must be approved for both 4.6m high and car carrier travel.

PART 4 — GENERAL REQUIREMENTS

4.1 Direction of travel

Travel is permitted in both directions on all roads approved for travel under this Notice except where stated in Appendix 1, or where one way traffic only is permitted.

4.2 Copy of Notice must be carried

A copy of this Notice, **excluding Appendix 1**, must be carried in the driving compartment whenever the vehicle is operating under this Notice, and must be produced when requested to do so by a police officer or an authorised officer.

4.3 Load security

The load must be secured in accordance with the requirements of the *Road Transport (Mass, Loading and Access) Regulation 2005*. Therefore a load restraint system must be used which is capable of withstanding the forces which result if the laden vehicle were subjected to each of the following separately:

- (a) 0.8g deceleration in a forward direction;
- (b) 0.5g deceleration in a rearward direction;
- (c) 0.5g acceleration in either lateral direction; and
- (d) 0.2g acceleration in either from the load in a vertical direction:

where “g” (the acceleration due to gravity) is equal to 9.81 metres/second/second (Refer Diagram 4.1).

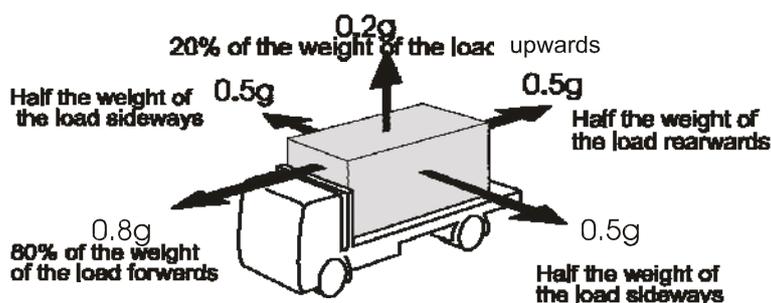


Diagram 4.1

Note: For additional guidance on load restraint refer to the National Transport Commission (NTC) *Load Restraint Guide*, Second Edition, 2004. Copies of the Guide can be obtained from Roads and Maritime Services, or may be accessed from the NTC website www.ntc.gov.au

PART 5 — SPECIAL REQUIREMENTS

5.1 Vehicles constructed to 4.6m carrying general freight

(i.e. freight other than vehicles, livestock, or primary produce)

- (a) A vehicle or vehicle combination that is constructed to exceed 4.3 metres in height, but does not exceed 4.6 metres in height and is carrying freight other than vehicles, cattle, sheep, pigs, wool, hay bales, or other primary produce, must;

- (i) have a maximum deck height of 1.2 metres over at least 50% of its deck length; and
- (ii) have air suspension on trailers; and
- (iii) be operated at 10% less than the gross mass limit applicable to the vehicle or combination.

5.2 Multi-deck stock crates

Livestock must not be carried in an upper deck of a multi-deck stock crate unless any lower deck of the stock crate is fully laden.

5.3 Car carriers

Car carriers operating under this Notice must comply with the following requirements.

- (a) Car carriers must not carry a load other than motor vehicles, trailers, and caravans.
- (b) The overall height of the car carrier and its load does not exceed 4.6 metres.
- (c) With the exception of cab/chassis, all vehicles carried as loading must be carried directly on decking fixed to the base vehicle structure and not carried on other vehicles being carried as loading by the base vehicle.
- (d) No vehicle can be carried on an upper deck unless all decks below it, except the well deck, are fully occupied, except under the following circumstances:
 - (i) Where the rear of the upper deck of the car carrier can be lowered, vehicles that cannot fit in the lower deck may be transported on the lowered section of the upper deck if the rest of the lower deck of the car carrier is full (refer Figure 5.1);

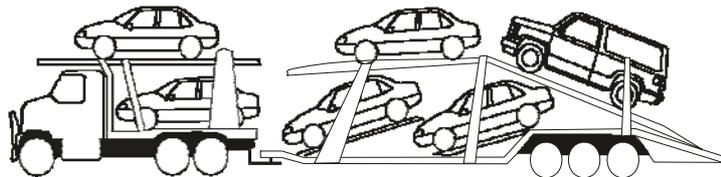


Figure 5.1

- (ii) a vehicle may be carried on the upper deck forward of the lowered upper deck if there is a vehicle in the space under it, or the vehicle is above the cabin of the car carrier (refer Figure 5.2). If the vehicle can fit into the lower deck, it must be carried on the lower deck.

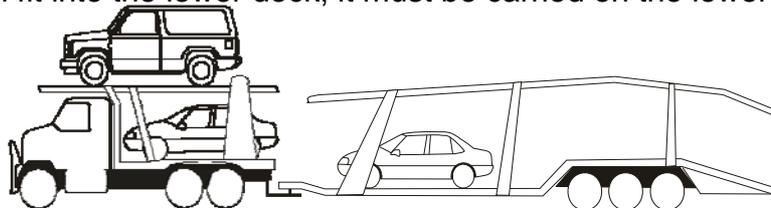


Figure 5.2

- (e) Where a mix of vehicles is carried and there is a difference in the mass of individual vehicles the total mass of the vehicles on the upper deck shall not exceed the total mass of the vehicles on the lower deck.
- (f) For all vehicles, except cab/chassis:

- (i) a secure and substantial part of the structure of each end of each vehicle carried must be lashed to the structure of the car carrier;
 - (ii) where wheel lashings are relied upon to meet this requirement at least one wheel on each side of each vehicle, must be secured to the car carrier.
- (g) For cab/chassis:
- (i) at least one end of each vehicle's chassis frame must be lashed to the structure of the car carrier;
 - (ii) the other end may be secured to the chassis frame of another cab/chassis if both ends of the lower cab/chassis are lashed to the structure of the car carrier.
- (h) Except for cab/chassis as above, the securing of one vehicle to another vehicle being carried on the same carrier, is not permitted.

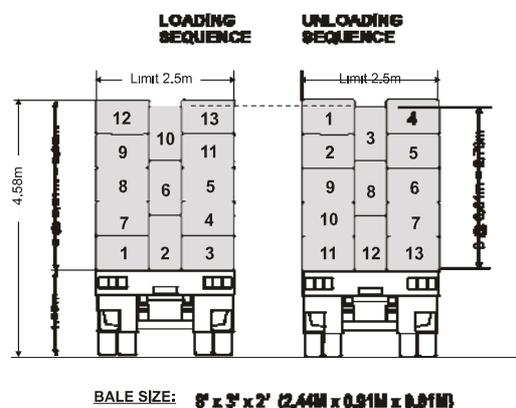
5.4 Hay bales

A vehicle or vehicle combination that exceeds 4.3 metres in height but does not exceed 4.6 metres in height that is transporting hay bales must comply with the load restraint requirements listed in either section 5.4.1 or 5.4.2.

5.4.1 Conventional Hay Bale Load Restraint

- (a) For hay bales 1.5 metres (or more) long, each group of bales laid transversely must be secured to the vehicle structure by webbing and webbing winches;
- (b) For hay bales less than 1.5 metres long laid transversely, at least every second group of bales and both end bale groups must be secured by webbing and webbing winches, and a cap tarpaulin must cover and secure the top of the entire load;
- (c) Gates must be provided at each end of the load and held upright by diagonal chains, or equivalent means, attached to the tie rails. (Ropes will not provide sufficient restraint for this purpose);
- (d) Angle or edge protectors must be placed between the webbing straps and the bales.

Note: For hay bales 1.5 metres (or more) long, the loading and unloading pattern and sequence described in Figure 5.3 should be followed to prevent accidents when loading and unloading the vehicle.



Load on 12.5m trailer: 13 x 5 = 65 bales
Figure 5.3

5.4.2 Certified Load Restraint System

A certified load restraint system compliant with the performance standards outlined in Section F and certified in accordance with Section I of the NTC *Load Restraint Guide*, Second Edition 2004.

A copy of the certification, including all relevant reports and diagrams must be carried in the vehicle at all times when operated at a height exceeding 4.3 metres and must be presented to an authorised officer when requested.

5.5 Wool bales

Wool bales carried under this Notice must comply with the following loading pattern.

(a) Loading Pattern

- (i) The first tier comprises two rows of bales placed on their flat with heads together and butts facing outwards. Bales shall be loaded so that no seam of any bale projects beyond the outermost limit of the tray of the vehicle (See Figure 5.4 Loading Pattern)

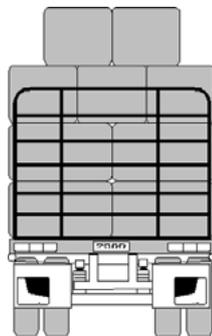


Figure 5.4

- (ii) The second tier is placed on top of the first tier in the same pattern.
- (iii) The third tier comprises three rows of bales placed on their flat with their ends facing to the front and back of the vehicle.
- (iv) The fourth tier comprises no more than two rows of binding bales placed on their flat, with at least one row facing to the front and back of the vehicle.
- (v) If the fourth tier comprises only one row of bales, it should be placed along the centre of the vehicle, with the ends of all bales facing the same direction, either outwards or to the front and back of the vehicle.

(b) Restraining the load laterally

- (i) The outward spread of the lower bales along the sides of the vehicle must be minimised by using separate lashings to restrain the centre of the lower two rows of bales (See Figure 5.5).

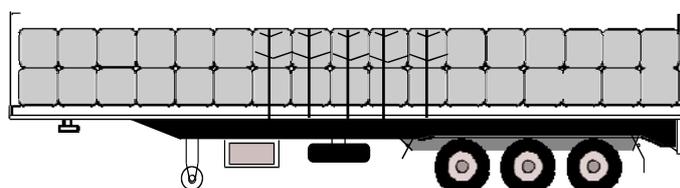


Figure 5.5

- (ii) Lashings (minimum standard 12 mm synthetic ropes) must be tied over the five centre bales.
- (iii) After the third row of bales is added, webbing lashings each of a minimum rated capacity of 2 tonnes must be applied at each bale along the length of the vehicle (See Figure 5.6). Ropes alone are not strong enough for this application. Two webbing lashings must be replaced by rope where there are no suitable winch positions, except for the end two lashings, provided that the two lashings are not side by side.

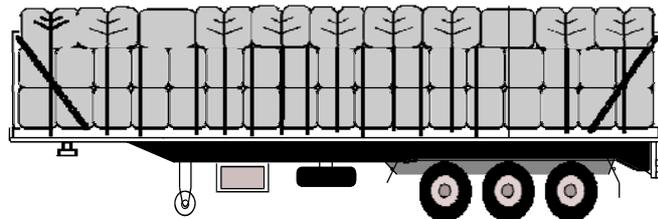


Figure 5.6

- (iv) Where a fourth row of two bales is added to the already restrained bottom three rows, it must be restrained by a suitable cap tarpaulin but still having ropes on the front and rear end bales (See Figure 5.7). If a cap tarpaulin is not utilised, ropes or webbing lashings should be applied at each bale along the length of the load.

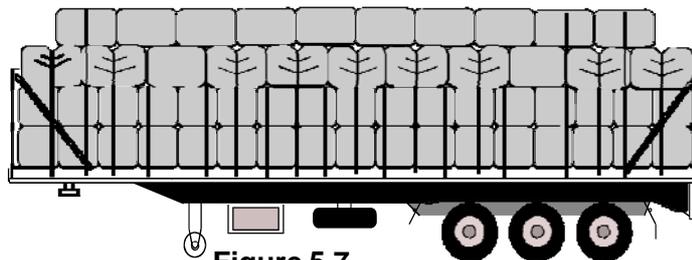


Figure 5.7

(c) Restraining the load forwards and rearwards

- (i) Forward and rearward movement of the load must be restrained by tie-down lashings, with additional restraint from strong head and tail gates.
- (ii) Diagonal chains of 2 tonnes minimum rated capacity must be attached to the top of these gates and onto the vehicle tie rail supports to provide additional restraint (See Figure 5.7).

5.6 Freight containers

- (a) Freight containers must not be carried under this Notice unless securely fastened to the carrying vehicle by twist locks.
- (b) Clause 5.6(a) applies whether a container is laden or unladen, and whether certified for marine use or not.

PART 6 - DEFINITIONS

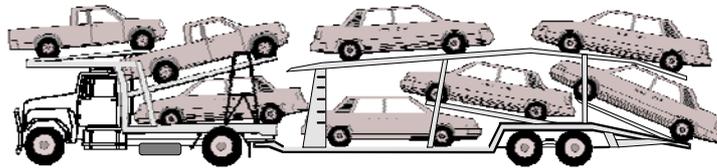
'cab/chassis' means an incomplete motor lorry without a body or turntable;

'lashing' means a device such as chain, cable or webbing together with suitable fittings which is used to restrain a load;

'primary produce' means:

- (b) fruit, grain, flowers, vegetables, tobacco, or farm or agricultural produce;
- (c) dairy, poultry, other bird farming, pig farming, bee keeping, oyster or fish culture produce;
- (d) nursery produce;
- (e) sheep, cattle or horses;
- (f) eucalyptus or other oil produce from leaves for distillation.

'well deck' means a deck area which is below the level of the top of the road wheels of the carrying vehicle.



Typical well-deck car carrier
