



**Government of South Australia**

Department for Transport,  
Energy and Infrastructure

MR 414 06/11

**Operating conditions  
when driving on roads in South Australia**

**This Code of Practice should be read in conjunction with  
the Information Guide for B-Doubles.**

# **Code of Practice for B-Doubles**



**June 2011**

This booklet is a legal document. You must comply with the conditions contained in it when operating B-Doubles.

<b>WHAT THE TERMS MEAN.....</b>	<b>1</b>
<b>WHAT THE TERMS MEAN (CONTINUED) .....</b>	<b>2</b>
Explanatory Note.....	2
<b>INTRODUCTION .....</b>	<b>3</b>
1 What is a Code of Practice?.....	3
2 What is a B-Double? .....	3
<b>USING THIS CODE.....</b>	<b>ERROR! BOOKMARK NOT DEFINED.</b>
3 When do you use this Code? .....	4
4 What documentation must you carry?.....	4
<b>ROUTE NETWORKS .....</b>	<b>5</b>
5 The Road System in South Australia .....	5
6 Approved routes.....	6
7 Special Conditions .....	6
8 Damage to roads .....	6
9 Roads and infrastructure.....	6
10 General route conditions.....	7
<b>DIMENSION LIMITS.....</b>	<b>7</b>
11 Width of Combination.....	7
12 Length of Combination.....	7
13 Height of Combination .....	7
14 Transporting Livestock at General Mass Limits .....	8
15 Transporting baled wool.....	9
16 Other dimension limits .....	9
<b>WARNING TO OTHER ROAD USERS.....</b>	<b>10</b>
17 Warning signs .....	10
18 Sign Size.....	10
19 Sign material .....	10
20 Sign wording .....	10
<b>MASS LIMITS .....</b>	<b>11</b>
21 Axle mass limits and spacing requirements .....	11
22 Drive axle on prime movers .....	11
23 Steer axle on prime movers .....	11
<b>COUPLING REQUIREMENTS.....</b>	<b>11</b>
24 Minimum rating for fifth wheel assemblies and king pins .....	11

<b>MAINTENANCE, ACCREDITATION AND INSPECTIONS.....</b>	<b>12</b>
25 Vehicles registered in South Australia .....	12
26 Vehicles registered in other States or Territories .....	12
<b>TRANSPORTING DANGEROUS GOODS .....</b>	<b>12</b>
27 Transporting Dangerous Goods.....	12
<b>HIGHER MASS LIMITS.....</b>	<b>13</b>
28 Configuration eligibility .....	13
29 Axle mass limits .....	13
30 Coupling Requirements .....	13
31 Mass Management Scheme .....	14
<b>TURNING REQUIREMENTS .....</b>	<b>15</b>
32 Intersections and junctions .....	15
33 Laned roads.....	15
34 Unlaned roads.....	16
35 Driveways and depots.....	17



## What the Terms Mean

**Approved Intelligent Transport System** means an intelligent transport system approved for the purposes of the IAP by Transport Certification Australia Limited (TCA).

**B-Double Approved Route Network** is a network system published via the DTEI RAVnet Online Mapping System or PDF maps published via the DTEI website that show the approved routes that B-Doubles can travel on when operating at either General Access Mass Limits or at Higher Mass Limits, or on Commodity routes.

**Depot** means a private property that abuts and has driveway access to and from a road.

**DTEI** means Department for Transport, Energy and Infrastructure.

**Explanatory Notes** contain information that does not form part of the conditions of this Code of Practice and is provided for explanation purposes only.

**General Access Vehicle (GAV)** is a vehicle that operates within the:

- axle mass limits specified in Table 1 of the *Road Traffic (Mass and Loading Requirements) Regulations 1999*; and
- mass limits relating to axle spacing in Section 3, Table 2, *Road Traffic (Mass and Loading Requirements) Regulations 1999*, and does not exceed:
  - a height of 4.3 metres
  - a length of 19 metres or
  - a total mass of 42.5 tonnes; and
- is not a Controlled Access Bus.

**General Mass Limits (GML)** means the:

- defined axle mass limits in Table 1 of the *Road Traffic (Mass and Loading Requirements) Regulations 1999*;
- mass limits relating to axle spacing in Section 3, Table 2 of the *Road Traffic (Mass and Loading Requirements) Regulations 1999*.

**General Access Dimension Limits** means the:

- dimension limits as specified in Division 2 – Dimensions of Part 7 Vehicle Configurations and Dimensions of the *Road Traffic (Vehicle Standards) Rules 1999*.
- but does not include any dimensions specified for a Controlled Access Bus, Road Train, B-Double or any other combination over 19.0m long.

**Higher Mass Limits** means axle group mass limits that are higher than the General Mass Limits (GML) specified in the *Road Traffic (Mass and Loading Requirements) Regulations 1999*, at which approved vehicle combinations can operate, if fitted with certified Road Friendly Suspensions.

**'Intelligent Access Program (IAP)'** means a program to allow heavy vehicles to have access, or improved access, to the road network in return for monitoring, by an approved intelligent transport system, of their compliance with specified access conditions.

**Maintenance Management Scheme (MMS)** means a scheme that is recognised by Department for Transport, Energy and Infrastructure (DTEI) as meeting the requirement of the maintenance management module (including audit requirements) of the National Heavy Vehicle Accreditation Scheme (NHVAS).

## What the Terms Mean (Continued)

**Mass Management Scheme** means a scheme that is recognised by the DTEI as meeting the requirements of the mass management module of the NHVAS.

**National Heavy Vehicle Accreditation Scheme (NHVAS)** means the comprehensive accreditation package developed by the then National Road Transport Commission (now known as the National Transport Commission NTC) and approved by the Australian Transport Council on 14 November 1997.

**Valid National Heavy Vehicle Accreditation Scheme (Mass Module) label** means a label:

- issued to a scheme member (whose accreditation status has not been suspended or cancelled); and
- that is legible and displayed on the nominated vehicle for which the label was originally issued by the accrediting authority.

**Restricted Access Vehicle (RAV)** means a vehicle that exceeds either the General Mass Limits or the General Access Dimension Limits contained in the Road Traffic Act 1961 and Regulations and can only travel on approved routes.

**RAVnet Online Mapping System** is an interactive online map system, managed by DTEI, that displays approved heavy vehicle route networks available in South Australia. Restrictions associated with specific route networks are also included.

**Road Friendly Suspension** means a suspension system that has been certified as meeting the performance criteria set out in the Department of Infrastructure, Transport, Regional Development and Local Government (DITRD LG, formerly DOTARS) *Vehicle Standards Bulletin 11 – ‘Certification of Road Friendly Suspension Systems, July 2004.*

### Explanatory Note

*For further information relating to Higher Mass Limits, refer to the ‘Higher Mass Limits’ section of this Code of Practice.*

## Introduction

### 1 What is a Code of Practice?

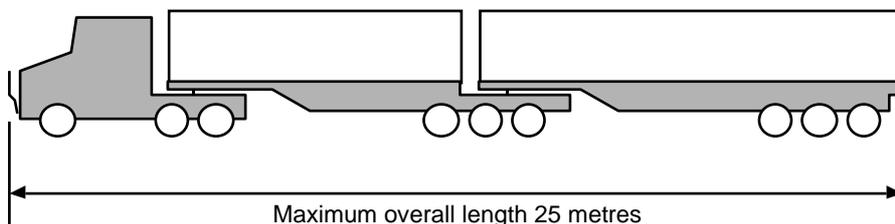
- 1.1 This Code of Practice for B-Doubles is a legal document issued by DTEI under a Notice of Approval and Exemption published in the South Australian Government Gazette entitled "Operation of B-Double Vehicles up to 25m in Length".
- 1.2 This Code sets out all the requirements, in addition to the *Road Traffic Act 1961* and Rules and Regulations, that you, as an owner, driver or operator of a B-Double must comply with when you are operating the vehicle on South Australian roads.
- 1.3 It is your responsibility to make sure that you are operating your vehicle in accordance with this Code of Practice.
- 1.4 This Code of Practice does not replace the responsibility that you have as the owner, operator or driver, to comply with the *Road Traffic Act 1961* and Rules and Regulations and Local Government by-laws.
- 1.5 You commit an offence if you do not comply with this Code or the *Road Traffic Act 1961*, Rules and Regulations.

#### Explanatory Note

*For a full understanding of how to operate B-Doubles in South Australia, you should read this Code of Practice in conjunction with the "Information Guide for B-Doubles".*

### 2 What is a B-Double?

- 2.1 For the purposes of this Code of Practice, a B-Double is a vehicle combination consisting of a prime mover towing two semi-trailers where:
  - 2.1.1 the first semi-trailer is connected to the prime mover by a fifth wheel coupling; and
  - 2.1.2 the second semi-trailer is connected to the first semi-trailer by a fifth wheel coupling; and
  - 2.1.3 the constructed overall length of the combination does not exceed 25 metres, unless permitted under the conditions of a separate Notice.



## Using this Code

### 3 When do you use this Code?

A B-Double that exceeds one or any combination of the undermentioned dimensions and mass is classified as a Restricted Access Vehicle (RAV) and must operate under the operating conditions detailed in this “Code of Practice”.

- 4.3 metres in height;
- a gross mass of 42.5 tonnes;
- an overall length of 19.0 metres.

### 4 What documentation must you carry?

- 4.1 From 1<sup>st</sup> July 2011, when operating under this Code, you must carry a legible, current and complete copy of the relevant B-Double Gazette Notice(s) either in hardcopy or in an electronic format so that it can be read from a device having a visual display (e.g. smart phone, tablet or laptop PC, in vehicle unit) that is carried within the vehicle and that is accessible, to a DTEI Authorised Officer appointed under the *Road Traffic Act 1961* or a Police Officer, from outside the vehicle.
- 4.2 When operating a B-Double at Higher Mass Limits and not operating under IAP, you may also be required to carry a completed Route Compliance Certificate. Refer to Section 31 – *Mass Management Scheme* for further details. You must produce this document if you are requested by an Authorised Officer appointed under the *Road Traffic Act 1961* or Police Officer.
- 4.3 You are not required to carry a copy of this Code.

#### Explanatory Note

*Copies of the Gazette Notice, Code of Practice, Information Guide, and Approved Route Network Maps for GML and HML B-Double Vehicles are available on DTEI’s website.  
The website can be found at [www.sa.gov.au/heavyvehicles](http://www.sa.gov.au/heavyvehicles)*

### 5 The Road System in South Australia

The standard of the road system in South Australia varies significantly from area to area. Some roads, such as main highways and key arterial roads have been designed to carry large and heavy vehicles while other roads provide access to residential areas and are generally only designed for vehicles with a mass of 4.5 tonnes or less.

The Road Traffic Act 1961 provides two categories for the operation of heavy vehicles on South Australian Roads. The two categories are:

- General Access Vehicles; and
- Restricted Access Vehicles.

General Access Vehicles (GAVs) are vehicles that operate within specified mass and dimension limits. These limits provide general protection for the whole of the road system in South Australia and allows these vehicles to operate on the road network without any route or time restrictions other than locally imposed controls, such as load limits on bridges.

The Restricted Access Vehicle (RAV) category allows larger and heavier vehicles to operate on South Australia's road system, but restricts access to approved routes that have been designed with the strength and capacity to cater for this size of vehicle. B-Doubles are classified as RAVs.

## Route Networks (Continued)

### 6 Approved routes

- 6.1 B-Doubles operating at General Mass Limits must only operate on the approved route network(s) published via the DTEI RAVnet Online Mapping System or PDF maps published via the DTEI website titled '*Route Network for B-Double Vehicles up to 25m in Length General Mass Limits (GML)*'.
- 6.2 If operating at Higher Mass Limits, you must only drive on the approved route network(s) published via the DTEI RAVnet Online Mapping System or PDF maps published via the DTEI website titled '*Route Network for B-Double Vehicles up to 25m in Length Higher Mass Limits (HML)*'.
- 6.3 If carrying a defined commodity, you may operate at General Mass Limits on the approved route network(s) published via the DTEI RAVnet Online Mapping System or PDF maps published via the DTEI website titled '*Commodity Routes for B-Doubles*'.
- 6.4 The approved route network maps may contain special conditions relating to travel on particular roads and bridges, as well as turning requirements or restrictions for some intersections and junctions.
- 6.5 PDF Maps are available on DTEI's website at [www.sa.gov.au/heavyvehicles](http://www.sa.gov.au/heavyvehicles) at this time, however the RAVnet Online Mapping System will replace this existing system and eventually PDF maps will no longer be made available.

### 7 Special Conditions

- 7.1 When a B-Double is travelling along Main South Road between Cape Jervis and Delamere, the drive axle group of a tandem drive prime mover shall have a mass of not less than 14 tonnes when the B-Double is carrying a load of more than 10 tonnes.
- 7.2 After disembarking from the ferry at Cape Jervis, the B-Double must allow all other passenger vehicles disembarking from the ferry to proceed towards Myponga before the B-Double proceeds.

### 8 Damage to roads

- 8.1 You must make sure that there is enough side and overhead clearance on the roads that you intend to use.

### 9 Roads and infrastructure

- 9.1 The conditions set out in this document may be overridden at any time by a limitation sign fixed on a bridge, culvert, causeway, road or road ferry.

#### Explanatory Note

*Some parts of the road system, such as bridges, overhead services and wires, roadside furniture (signs, traffic lights etc.) and vegetation may not allow sufficient clearance for a vehicle and load travelling under this Code.*

*Section 107 of the Road Traffic Act 1961 (Damage to Road Infrastructure) requires you to notify the Police, DTEI or Council, if you cause damage to roads, bridges and culverts, or if you interfere with roadside furniture without reasonable excuse.*

*If you cause or permit anything to fall on the road that may hinder or injure other road users (traffic signs, tree branches etc.) Australian Road Rule 293 requires you to remove it, or take action to have it removed.*

## Route Networks (Continued)

### 10 General route conditions

- 10.1 A 25 metre long B-Double can only operate on the routes that have been approved for a 25 metre B-Double.
- 10.2 If your B-Double does not exceed 23 metres in length, you can operate on the routes that are approved for both a 23 metre and a 25 metre B-Double.
- 10.3 You must not assemble or disassemble your B-Double on any approved route except under the following circumstances:
- 10.3.1 if broken down; or
  - 10.3.2 in order to proceed on a temporary by-pass around a road blockage; or
  - 10.3.3 as reasonably required to remedy a breach of mass, dimension or load restraint requirement identified by an Authorised Officer appointed under the *Road Traffic Act 1961* or a Police Officer.
- 10.4 If you intend using a roadside parking area you can only stop in a parking area showing a 'Rest Area' sign or a 'Truck Parking Area' sign. Parking areas can only be used for rest purposes or vehicle checks but not for assembly or disassembly purposes except in accordance with 10.3.
- 10.5 B-Double vehicles are prohibited from using rest areas indicated with the 'Unsuitable for Trucks' sign. This sign displays a symbolic articulated vehicle with a diagonal 'Ban' slash across the vehicle. All signs display white symbols and letters on a blue background.
- 10.6 All entrances into and exits out of a depot must be suitable for B-Double operation (See Section 35).

## Dimension Limits

### 11 Width of Combination

- 11.1 The maximum overall width of your B-Double, including any load, must not exceed 2.5 metres.

### 12 Length of Combination

- 12.1 The maximum overall length of your B-Double must not exceed 25 metres, unless permitted under a separate Notice.

### 13 Height of Combination

The maximum overall height of the combination including any load must not exceed 4.3 metres except under the following loading conditions:

#### 13.1 Transporting vehicles as the load to a height of 4.6 metres

You can carry vehicles up to an overall height of 4.6m provided:

- 13.1.1 the B-Double is constructed as a multiple deck vehicle transporter and does not exceed a constructed height of 4.3 metres; and
- 13.1.2 vehicles are not loaded on the upper deck unless each deck below is fully loaded; and
- 13.1.3 the load is made up solely of vehicles.

#### Explanatory Note

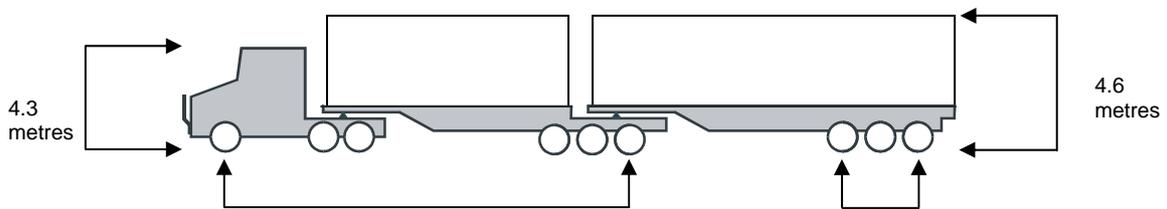
*For the purpose of this clause, vehicles include trailers and caravans.*

## Dimension Limits (Continued)

### 13.2 Semi-trailers built to a fixed height up to 4.6 metres (for carrying low density freight).

Your semi-trailer, except those constructed to carry vehicles, may be built to a fixed overall height of 4.6 metres providing:

- 13.2.1 at least 50% of the deck of each semi-trailer is no more than 1.2 metres above the ground; and
- 13.2.2 each semi-trailer axle group is fitted with an air suspension system and effective dampers; and
- 13.2.3 the gross mass of the prime mover and the 1st semi-trailer combination must not be more than 90% of the gross mass limit that applies to that combination, based on the axle mass limits specified in the Mass Loading Regulations Part 1, Table 1; and
- 13.2.4 the mass on the axle group of the 2nd semi-trailer must not be more than the 90% of the axle mass limit that applies to that axle group as specified in the Mass & Load Regulations Part 1, Table 1.



General access mass limits for prime mover & 1<sup>st</sup> semi trailer is 42.5 tonnes.

The mass limit for a fixed height above 4.3 metres is 38.25 tonnes.

General access mass limits for semi-trailer is 20 tonnes.

The mass limit for a fixed height above 4.3 metres is 18 tonnes

## 14 Transporting Livestock at General Mass Limits

- 14.1 You can carry livestock on your B-Double, providing you meet all of the following requirements:
  - 14.1.1 the height of your vehicle and load does not exceed 4.6 metres;
  - 14.1.2 the prime mover has a tandem drive axle group;
  - 14.1.3 the first semi-trailer has a tri-axle group towards the rear and the second semi-trailer has either a tandem or tri-axle group towards the rear;
  - 14.1.4 your load is made up only of livestock, these being cattle, sheep, pigs and goats.
  - 14.1.5 if any decks on your vehicle are not fully loaded, the animals must be confined in a full width compartment that is fully loaded;
  - 14.1.6 animals are not loaded on an upper deck until each deck below is fully loaded.

### Explanatory Note

*If your semi-trailer was built after 1 July 1988, you must obtain approval from the Australian Motor Vehicle Certification Board in order for it to be constructed above 4.3 metres.*

*The Australian Design Rule Compliance Plate fitted to the semi-trailer must be marked with the approval number.*

## Dimension Limits (Continued)

### 15 Transporting baled wool

15.1 You can carry baled wool on your B-Double provided:

- 15.1.1 the baled wool is not loaded more than 4 layers high; and
- 15.1.2 the maximum overall height of your vehicle, including the baled wool, does not exceed 4.6 metres; and
- 15.1.3 the overall width of the axles or axle groups of the vehicle that carries the load (excluding the front axle or axle group) must not be less than 2.1 metres, when measured between two vertical parallel planes located at the outermost point of the tyres.

### 16 Other dimension limits

16.1 You must meet all other dimension requirements that are set out in the *Road Traffic (Vehicle Standards) Rules 1999*, Part 7, Division 2.

#### **Explanatory Note**

*The "Information Guide for B-Doubles" summarises the mass, dimensions and axle spacing requirements relating to the operation of B-Doubles.*

*The Road Traffic Act 1961 and Regulations contains full details.*

*You will need to refer to either the Information Guide or the legislation referred to above for further information.*

## Warning to Other Road Users

### 17 Warning signs

- 17.1 If your B-Double exceeds an overall length of 22 metres, you must display a "LONG VEHICLE" warning sign that:
- 17.1.1 can be clearly seen by other road users.
  - 17.1.2 is fitted horizontally at the rear of the combination.
  - 17.1.3 is not more than 1.8 metres from ground level; or
  - 17.1.4 is not less than 500 millimetres from ground level.
  - 17.1.5 if the warning sign is in two (2) parts, the parts must be fitted horizontally to your B-Double, at the same height.

### 18 Sign Size

- 18.1 The "LONG VEHICLE" warning sign on your B-Double must be at least 1.2 metres wide and 250 millimetres high.

### 19 Sign material

- 19.1 Your B-Double warning sign must:
- 19.1.1 be made from 1 or 2 parts of sheet steel or a similar material that is 0.8 millimetres thick; and
  - 19.1.2 be coated with yellow Class 1 or 2 retro-reflective material that meets Australian Standard AS 1906, "Retro-Reflective Materials and Devices for Road Traffic Control Purposes"; and
  - 19.1.3 have a black border; and
  - 19.1.4 show the sign maker's name or logo and the class of retro-reflective material in block letters that should be no more than 10 millimetres high.

### 20 Sign wording

- 20.1 The warning sign on your B-Double must show the words:
- 20.1.1 "LONG VEHICLE" in black capital letters at least 180 millimetres high; and
  - 20.1.2 be written in typeface Series B (N) that complies with Australian Standard AS 1744 Forms of Letters and Numerals for Road Signs.
- 20.2 If your warning sign is in two (2) parts, "LONG" must be on one part of the sign and "VEHICLE" on the other part.

## Mass Limits

### 21 Axle mass limits and spacing requirements

- 21.1 When operating at General Mass Limits your B-Double must comply with the *Road Traffic (Mass and Loading Regulations) 1999*, Schedule 1, 'Mass and Loading Requirements for Heavy Vehicles' Table 4, 'Mass Limits Relating to Axle Spacing (B-Doubles)'. This information is detailed in the "Axle Spacing and Mass Formula Chart" of the 'Information Guide for B-Doubles'.
- 21.2 If you are operating your B-Double at Higher Mass Limits, you are only required to comply with the axle spacing schedule as it relates to General Mass Limits and B-Double spacing requirements noted in 21.1 above.

#### Explanatory Note

*The "Information Guide for B-Doubles" summarises the mass, dimensions and other spacing requirements relating to the operation of B-Doubles. The Road Traffic Act 1961 and Regulations contains full details.*

*You will need to refer to either the Information Guide or the legislation referred to above for further information.*

### 22 Drive axle on prime movers

- 22.1 If you operate your B-Double at a Gross Combination Mass greater than 42.5 tonnes, the prime mover must be fitted with a tandem drive rear axle group.
- 22.2 If inter-axle differentials are fitted, they must be of the positive locking type.

### 23 Steer axle on prime movers

- 23.1 Prime movers with an approved Front Underrun Protection Device (FUPD) may operate at up to 6.5 tonnes provided they comply with requirements under the SA Government Gazette Notice titled '6.5 Tonne Steer Axle Mass Limit for Heavy Vehicles'.

## Coupling Requirements

### 24 Minimum rating for fifth wheel assemblies and king pins

- 24.1 For B-Doubles operating up to a Gross Combination Mass of 62.5 tonnes, all fifth wheel assemblies and fifth wheel king pins must have a "D" rating equal to or greater than 112 kilonewtons.

#### Explanatory Note

*For B-Doubles operating at Higher Mass Limits above 62.5 tonnes, higher "D" ratings are required.*

*Refer to the "Higher Mass Limits Section" on pages 17 to 18 of this Code for further information.*

## Maintenance, Accreditation and Inspections

### 25 Vehicles registered in South Australia

25.1 Where your B-Double (prime mover and trailers) is registered in South Australia, you are required to either:

25.1.1 operate under an approved Maintenance Management Scheme - each vehicle unit is required to show a label that clearly states accreditation and scheme membership;

or

25.1.2 complete an annual inspection - each vehicle unit must then show the appropriate current DTEI inspection label that clearly states:

25.1.2.1 the type of vehicle configuration; and

25.1.2.2 the inspection expiry date (month and year).

#### Explanatory Note

*Accreditation labels for the prime mover are fixed to the right hand side door of the prime mover or sleeper cab. Accreditation labels for the trailer are located at the rear of the vehicle and as close as practicable to the number plate or registration disc.*

*DTEI Inspection labels for the prime mover are located on the left hand side face of the chassis, just in front of the front axle of the drive axle group. On the trailer, DTEI Inspection labels are located on the left hand side of the main frame rail, just forward of the rear axle group.*

### 26 Vehicles registered in other States or Territories

26.1 If your B-Double is registered in New South Wales, ACT, Victoria, Tasmania, Queensland or Western Australia, you do not need to show South Australian labels or undergo inspections that are over and above those required in your home state.

26.2 If your B-Double is registered in the Northern Territory, you must either:

26.2.1 show South Australian inspection labels; or

26.2.2 show Northern Territory inspection and rating labels; or

26.2.3 be accredited under an approved Maintenance Management Scheme (NHVAS) and show an appropriate current accreditation label.

## Transporting Dangerous Goods

### 27 Transporting Dangerous Goods

27.1 The transportation of dangerous goods by road is regulated by the *Australian Code for the Transport of Dangerous Goods by Road and Rail*, known as the ADG Code. It is the responsibility of the owner/operator to ensure that they are complying with the legal requirements detailed in this Code.

## Higher Mass Limits

### 28 Configuration eligibility

28.1 To be eligible for Higher Mass Limits (HML), your B-Double must have:

- 28.1.1 a prime mover that is fitted with a tandem drive axle group; and
- 28.1.2 semi trailers that are fitted with either a tandem or triaxle group; and
- 28.1.3 certified Road Friendly Suspensions fitted to axle groups that are operating at Higher Mass Limits.

#### Explanatory Note

*Higher Mass Limits (HML) refers to the scheme where approved vehicle combinations can operate at axle Group limits higher than those specified in the Road Traffic (Mass and Loading Requirements) Regulations 1999, provided the axle groups have been fitted with certified Road Friendly Suspensions.*

### 29 Axle mass limits

29.1 The overall mass on the individual axle groups of your B-Double must not exceed the limits listed in Table 1 below. These limits only apply to axle groups fitted with certified Road Friendly Suspensions.

Table 1

Axle or axle group configuration	Higher Mass Limit (tonnes)
Tandem axle groups fitted with dual tyres (8 tyres)	17.0
Tri-axle group fitted with dual tyres (12 tyres)	22.5

29.2 When operating your B-Double at HML, the overall mass on any axle group not fitted with Road Friendly Suspension must not exceed the Concessional Mass limits listed in Table 2 below.

Table 2

Axle or axle group configuration	Concessional Mass Limit (tonnes)
Tandem axle groups fitted with dual tyres (8 tyres)	17.0
Tri-axle group fitted with dual tyres (12 tyres)	21.0

29.3 A vehicle must not be loaded in excess of:

- (i) the manufacturers rating for axle, suspension, tyre or coupling capacities; or
- (ii) the manufacturer's Gross Vehicle Mass and Gross Combination Mass ratings; or
- (iii) the Gross Vehicle Mass and/or Gross Combination Mass as shown on the certificate of registration for that vehicle.

#### Explanatory Note

*Where your B-Double combination is fitted with only tandem axle groups, you can operate at Higher Mass Limits and are not required to be in a Mass Management Scheme, provided the axle groups are fitted with certified Road Friendly Suspensions.*

*Axle groups not fitted with certified Road Friendly Suspensions are not eligible to operate at Higher Mass Limits.*

### 30 Coupling Requirements

30.1 If you operate your B-Double at HML, you must fit fifth wheel couplings and king pins with "D" ratings equal to or greater than 119 kilonewtons.

## Higher Mass Limits (Continued)

### 31 Mass Management Scheme

- 31.1 For operation at HML, B-Doubles fitted with triaxle groups must be accredited under the Mass Management Module of the National Heavy Vehicle Accreditation Scheme.
- 31.2 For operation at HML, the prime mover must display a valid National Heavy Vehicle Accreditation Scheme (Mass Module) label.
- 31.3 All triaxle trailers in the B-Double combination, when loaded in accordance with the Higher Mass Limits in Table 1, must display a valid National Heavy Vehicle Accreditation Scheme (Mass Module) label where display labels on trailers is a requirement of the accrediting agency.
- 31.4 Drivers of B-Doubles accredited in South Australia who are **not** operating under the IAP must carry documented evidence (which may include a Route Compliance Certificate) of route planning, confirming that the higher mass limit route is available by reference to the B-Double HML approved network, in accordance with Standard 5 of the Mass Management Module of the National Heavy Vehicle Accreditation Scheme and shall produce the signed documented evidence if requested by an Authorised Officer appointed under the Road Traffic Act 1961, or a Police Officer. Where an alternative to the route compliance certificate is produced as evidence of route compliance that documentation must include axle and gross weight of the vehicle combination, the route to be traversed, be dated and signed by the driver.
- 31.5 Vehicles accredited in states and territories other than South Australia under the scheme specified in 31.1 that are **not** operating under the IAP must operate in South Australia under the terms and conditions in the 'Code of Practice for B-Doubles' book and on the route network(s) published via the DTEI RAVnet Online Mapping System or PDF maps published via the DTEI website titled '*Route Network for B-Double Vehicles up to 25m in Length Higher Mass Limits (HML)*'; provided the driver:
- 31.5.1 has determined that the higher mass limit route is available for the entire route being travelled by reference to the B-Double HML network; and
  - 31.5.2 records the route details on the DTEI Higher Mass Limit Route Compliance Certificate and has signed the Certificate; and
  - 31.5.3 carries the Route Compliance Certificate in the vehicle when operating at higher mass limits; and
  - 31.5.4 produces the Certificate if requested by an Authorised Officer appointed under the *Road Traffic Act 1961*, or a Police Officer; and
  - 31.5.5 the scheme member retains the original copy of the Certificate for a period of not less than 12 months from the date it is signed for auditing purposes.

#### Explanatory Note

*Vehicles that are accredited in other states are eligible to operate in South Australia in accordance with mutual recognition principles.*

*Copies of Route Compliance Certificates are available from DTEI's website at:*  
**[www.sa.gov.au/heavyvehicles](http://www.sa.gov.au/heavyvehicles)**

*Vehicles operating under the IAP are not required to carry a Route Compliance Certificate.*

## Turning Requirements

### Explanatory Note

Continuous arrows indicate that you must, wherever practicable, turn your vehicle in the direction shown.

Broken arrows indicate that you can only turn in the direction shown if it is impracticable to do otherwise, and only if it is safe to do so.

## 32 Intersections and junctions

32.1 When you are turning:

- 32.1.1 right from a one-way road, you must approach the intersection or junction as near as practicable to the right boundary of the one-way road (Fig 1);
- 32.1.2 right into a one-way road, you must enter as near as practicable to the right boundary of the one-way road (Fig 2);
- 32.1.3 left from a two-way road into a two-way road, you must keep as close as practicable to the left kerb (Fig 3).

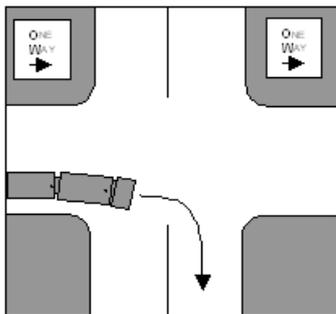


Fig 1

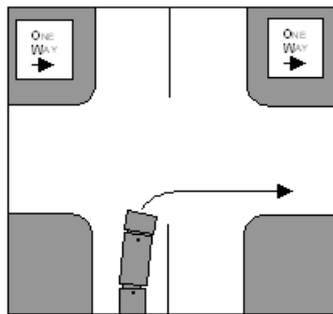


Fig 2

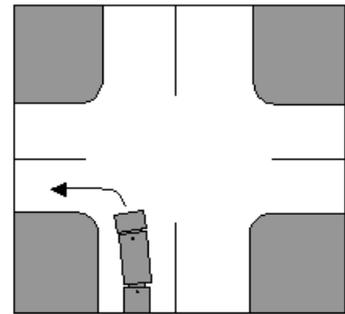


Fig 3

## 33 Laned roads

33.1 When you are operating on a laned road where turning arrows are marked you must:

- 33.1.1 turn only in the direction of the arrows marked on the road (Fig. 4);
- 33.1.2 turn only in the direction of the marked arrows and enter the corresponding lane of the other road (Fig. 5).

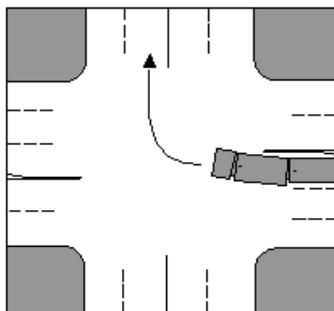


Fig 4

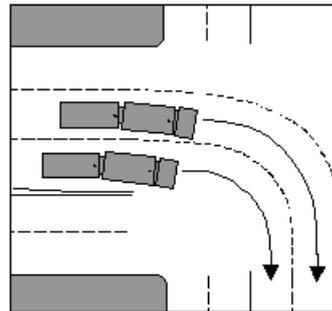


Fig 5

## Turning Requirements (Continued)

33.2 When you are operating on a laned road and turning lanes are provided as an extension of lane lines, you must keep the B-Double in the turning lane while you are turning (Fig. 5).

33.2.1 when you are operating on a laned road and you are turning right from a two-way road into a two-way road, you must not obstruct traffic coming from the opposite direction.

33.3 When you are operating on a laned road where turning arrows are not marked, and you are turning:

33.3.1 right from a laned road, you must begin your turn from the lane that is just to the left of the centre of the road (Fig. 6);

33.3.2 right into a laned road, you must where practicable, enter the lane nearest to the centre of the road (Fig.6);

33.3.3 left from a laned road, you must begin your turn from the left-hand lane that is as near as practicable to the left side of the road (Fig 7). Alternatively, if your vehicle displays a "DO NOT OVERTAKE TURNING VEHICLE" sign, you can turn from a marked lane next to the left lane (Fig 7a).

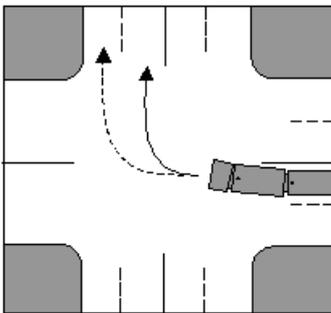


Fig 6

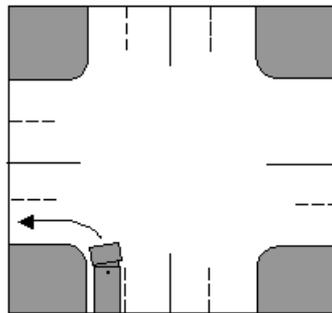


Fig 7

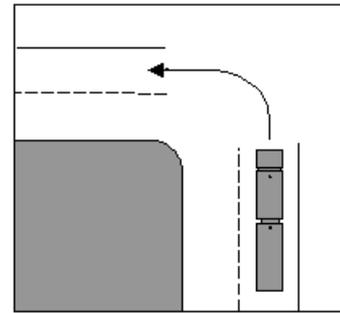


Fig 7a

## 34 Unlaned roads

34.1 When you are operating on an unlaned road and you are turning right from a two-way road into a two-way road you must, where practicable (Fig 8):

34.1.1 approach the intersection or junction to the left of and parallel to the centre of the road;

34.1.2 pass to the right of the centre of the intersection;

34.1.3 enter the other road just left of the centre.

34.2 When you are operating on an unlaned road and you are turning right from a two-way road into a two-way road, you must not obstruct traffic coming from the opposite direction.

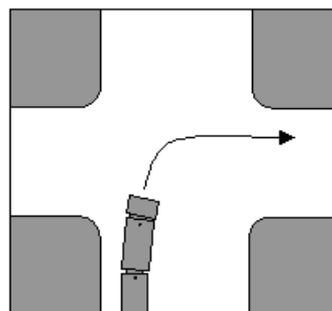


Fig 8

## Turning Requirements (Continued)

### 35 Driveways and depots

35.1 You must not reverse your B-Double into or out of roadways or depots.

35.2 When you are operating on a laned road and turning:

35.2.1 left into a depot, you must turn from the left lane (Fig 9). Alternatively, if your vehicle displays a "DO NOT OVERTAKE TURNING VEHICLE" sign, you can turn from a marked lane next to the left lane (Fig 9a);

35.2.2 right into a depot, you must turn from the lane that is nearest to the centre of the road (Fig 9);

35.2.3 left out of a depot, you must, where practicable, enter the lane that is nearest the left kerb (Fig 10);

35.2.4 right out of a depot, you must, where practicable, enter the lane that is nearest the centre of the road (Fig 10).

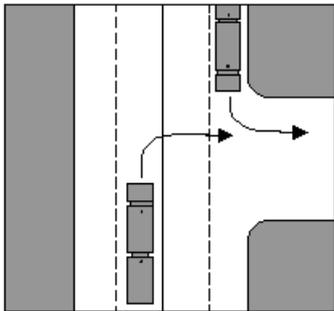


Fig 9

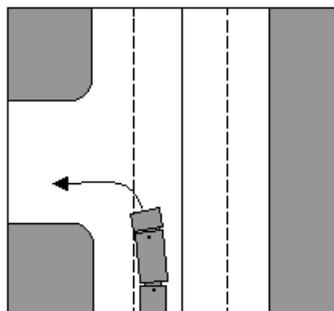


Fig 9a

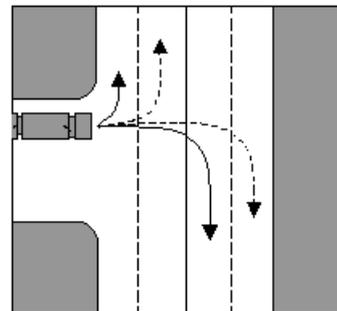


Fig 10

35.3 When you are operating on an unlaned road and turning:

35.3.1 left into and/or out of a depot, you must keep as near as practicable to the left kerb (Fig. 11);

35.3.2 right into a depot, you must approach the entrance to the left of, parallel to and as near as practicable to the centre of the road (Fig. 12);

35.3.3 right out of a depot, you must enter the road by moving to the left of the centre of the road (Fig.12).

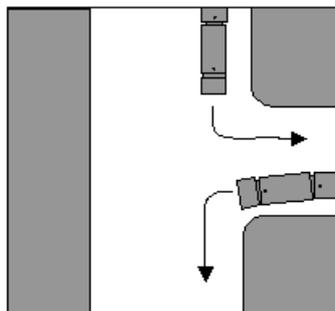


Fig 11

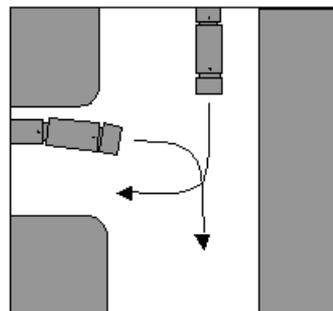


Fig 12